



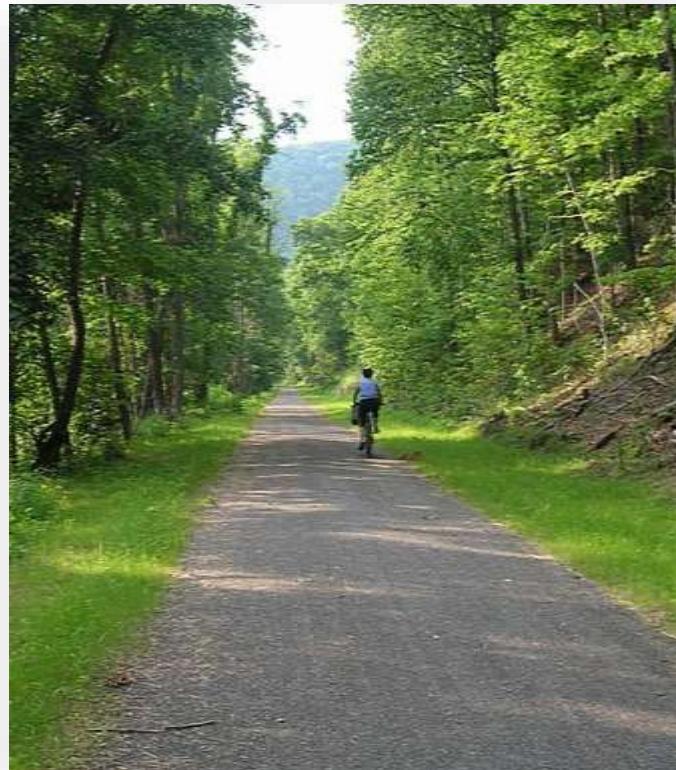
FRIENDS OF THE DOVER GREENWAY

Dover Town Meeting

May 1, 2017



The Dover Greenway Vision





Protections for Dover Residents

- No request for Town funds (FDG will gift to Town)
- Limits usage volume by restricting scope of trail
- Prevents loss of land to other potential lessees
- Removes rotting, contaminated ties / caps rail bed
- Dover does not assume responsibility for historical environmental liability

Did you know.....

There are over 60 rail trails just in MA and an additional 68 projects in progress totaling 300 miles of rail trails.

www.railstotrails.org/our-work/united-states/massachusetts/

Environmental liability

- ✓ Possibility of Dover rail bed contamination requiring soil clearance is extremely low (rural trail, no visible or recorded contamination)
- ✓ Legal responsibility for residual soil contamination remains with prior owners (Mass law 21E)
- ✓ Responsibility for contamination that occurs during rail trail construction covered by environmental insurance policy
- ✓ Best Practice to address existing rail bed is remove the ties, cap the bed with stone dust (Mass DEP) = a rail trail



Environmental Improvement

- **Licensed Site Professionals Letter**
 - Each LSP resides in Dover
 - “Environmental Plus”
- **MA Department of Environmental Protection**
 - Thomas Mahin and James Persky, Drinking Water regional contacts*
 - Environmental conditions would improve with rail trail
- **Colonial Water**
 - Phone conversation with Alan Melancon, November 17, 2015*
 - No history of water quality concerns

Letters to the Editor

The Dover Greenway is an environmental plus

Friends of Dover are currently considering a proposal to convert the rail trail between Dover and Needham into a mixed-use rail-trail conversion. The Dover Greenway (“DGV”) developed a Best Management Practice document, the “BMP”, to provide guidance for rail-trail conversions. The BMP provides a practical approach that is also protective of health and the environment.

Some citizens have suggested environmental concerns associated with the former railroad could result in increased risk to health or environmental risks. As registered Massachusetts Licensed Site Professionals (LSPs) with more than 15 years of combined experience addressing environmental contamination issues, and as Dover residents, it is our opinion that these fears are groundless and not borne out by the facts of rail-trail conversions elsewhere in Massachusetts.

Rail-trail conversions are semi-sealed areas. Like Dover, we analyze transportation and environmental impacts to Census tract with low traffic. As such, the contaminants likely to be associated with the corridor are similar to the materials listed on and adjacent to our roads. The presence of these contaminants, if typically managed on roadway projects without the failure of public structures associated with rail trail projects and—in our opinion, this increased concern regarding rail corridors is not based on risk, science or regulation.

While rail trail assessments started becoming common after 2000, the Massachusetts Department of Environmental Protection (“MassDEP”) developed a Best Management Practice document, the “BMP”, to provide guidance for rail-trail conversions. The BMP provides a practical approach that is also protective of health and the environment.

Rail-trail like the Dover scenario are categorized in the lowest risk category, which has “no contamination, pre-construction requirements, visual inspection and a MassDEP file review with pre-construction environmental testing recommended only if specific spill areas are identified.”

The relevant elements of the BMP have been incorporated into the feasibility study in the Beals & Thomas report commissioned by the town, which is available at on the town website www.dovermass.org/town-government/boards-commissions/rail-trail-commission.

These elements include removal of the existing concrete railroad ties that are the major contaminating source of contamination on the rail-trail bed followed by capping with a suitable barrier. These measures will effectively eliminate both a continuing source of contamination and prevent contact with contaminants in existing soils.

Since 2014, the BMP guidance has been successfully implemented on rail trail projects across the state and

Paul Laska of MassDEP, who developed the guidance, reported to us that he is not aware of any projects where problematic environmental contamination was discovered during or following the rail-to-trail conversion process in a rail-right-of-way environment such as Dover.

However, despite the low risk of environmental liability associated with the rail trail conversion, we understand that there will be potential environmental insurance policy to mitigate this already low risk. Thus, we are convinced that the proposed conversion will not represent an environmental liability for the town. It will also provide a net environmental benefit through removal of the treated railroad ties and by capping the rail-bed soils.

Because of these reasons, the Dover Greenway has our support. As fellow Dover residents, we believe this project has significant recreational benefits for our and for Dover's future generations and that it has no environmental downside.

In fact, it is an environmental plus since it is the only plan that addresses existing environmental conditions. We urge you to join us and vote “yes” for Article 18 on May 2.

Ronald E. Myrick, Jr.
PE, LSP, CMM

Paul McKinley,
PE, LSP

Nancy C. Roberts,
AE, LSP

*Appendix VIII, page 85 Beals & Thomas Dover Feasibility Study 12-15-15



A Trail for Everyone

- Respect all constituencies in trail design and planning
 - Low-maintenance, rural trail
 - Privacy screening
 - Retain horse trail crossings, protect with signage
 - Protects the hunt club tradition
- Safety, home values and crime data benefit all





No Financial Burden on Tax Payers

Projected cost

- Beals & Thomas estimate: \$1.2M including environmental insurance*

Maintenance of the trail

- \$50,000 maintenance escrow in estimate upfront (10 years projected expense)
- Friends of Dover Greenway will coordinate volunteer trail maintenance (60% of rail trails in US maintained by volunteers)

*Beals & Thomas, Feasibility Study Dover Recreational Path, Dover, MA December 17, 2015



If We Vote “NO” Tonight...

- Town of Dover misses opportunity to acquire this land;
 - Weston > DCR and Eversource acquired rail corridor
 - Sudbury > Eversource plan a 115-kilovolt (kV) transmission line
 - This has only occurred when towns did not lease their corridor
- Possibility of multi-town trail remains open (DCR, BCRTA)
- Missed opportunity to have a recreational resource accessible by all of Dover

*“55 percent of trail users exercising more now than before they had access to the trail”**

*Ross C. Brownson, “Promoting and Evaluating Walking Trails in Rural Missouri,” Saint Louis University School of Public Health, 1999



Benefits for all Dover Residents

- **Improve public safety, expand resources**
 - Safer in-town connections & recreation
 - Only ADA-accessible path allowing recreation for all
 - Environmental safeguard
- **Dover control of corridor**
 - Leverage town use
 - Economic value
- **Greater sense of community**
 - Connecting people and places



Be Part of a Legacy for Dover



FRIENDS OF THE DOVER GREENWAY

Thank you.

Please vote “YES” on Article 19